

Streetscape Plan

City of North Olmsted

2006

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I. Introduction

PURPOSE

The purpose of this document is to provide guidance for the site and structural development North Olmsted's commercial districts, specifically the public right of way and other public spaces. For more information regarding private property development, see the North Olmsted Design Guidelines. This document also identifies programs and initiatives to help the City organize and integrate the various individual land uses to create a more desirable destination for residents and visitors. The streetscape plan is intended to incorporate design solutions and approaches that will provide a unified and appropriate response to the district while not compromising uniqueness of each individual land owner.

CONTEXT

The City of North Olmsted's commercial area has grown into one of the region's top retail destinations. Currently, the North Olmsted retail district is characterized by a suburban pattern of commercial development and its role as a regional shopping and employment center. Development in the area has continued to evolve as older antiquated properties have been torn down and replaced with newer modern retail establishments. Historically, retail development in North Olmsted has grown in a western direction along Lorain Road. With the construction of Great Northern Mall, the area evolved into a retail hub, including significant commercial development in and around the triangle bounded by three major thoroughfares – Lorain Road, Great Northern Boulevard, and Brookpark Road. Newer construction has moved west of this area towards the proposed Crocker-Stearns Road extension and Interstate 480 interchange at Lorain Road. The construction of I-480 interchanges has allowed residents and shoppers a variety of easy access points into the commercial area.

Residential pockets are located along the rear property lines of the major commercial strip and are primarily builders' houses in a postwar suburban pattern. These residential areas are close enough to the retail strip to easily access shopping if neighborhood scale shopping existed. However, given the current development pattern of the district (road widths, scale, distance between businesses, building designs, curb-cuts, etc.), creating pedestrian friendly neighborhood shopping would be a challenge. However, investing in specific changes to the district can create a more pleasant and inviting place for residents and visitors.

Observations



Antiquated retail use



Antiquated retail area



Modern retail structure



Franchise architecture

Even though North Olmsted has many strengths and strong retail viability, the retail district's visual appearance is uncoordinated and chaotic. There is not one overriding or historic architectural style to the district. Older, smaller retail establishments are being replaced with modern retail businesses. As a result of this overall continued investment in retail redevelopment, the North Olmsted retail district, unlike many other inner ring Cleveland suburbs, continues to be a successful regional retail destination. The district is home to numerous national retail establishments. These exist as free standing structures or integrated into a shopping strip center or Great Northern Mall. However, for various reasons, there are still some outdated retail structures that have yet to be modernized to current retail trends. The juxtaposition of these older retail establishments against the newer more modern ones adds to some of the visual disorder along the retail corridor. Also, the majority of the newer retail establishments are defined by "franchise architecture." While many of these retail establishments, individually, are in good condition, they lack a cohesive singular architectural style. While the City is fortunate to have this vital retail district, it does create a challenge to the future design and planning of the area.

The City's retail district is a vehicular dominated environment. This always creates a challenge in developing an overall image that establishes a clearly organized and recognizable district with its own distinct feel or sense of place. Buildings vary widely in architectural character, scale, and condition and often appear randomly sited in relation to adjacent uses as well as to the street. The existing streetscape is dominated by vehicular traffic with minimal or discordant landscape treatments. The retail district would benefit from strong functional or aesthetic unity.

Signage Observations:

- § Changing regulations over time have resulted in many different types and styles of signs and create visual clutter.
- § The City lacks civic signage, including strong identifiers at City boundaries.
- § North Olmsted has little if any directional signage.

Frontage Area Observations:

- § The City has numerous overhead power lines that have a significant visual impact on the right-of-way.
- § Some existing landscaping is seriously compromised by high-tensions wires, especially street trees.
- § Much of the existing streetscape landscaping is in poor condition.
- § There is a lack of a uniform street edge, and in some areas, poor road edge conditions exist (i.e., crumbling curbs).
- § Brick edge pavers in the right-of-way are of varying colors, sizes, patterns, and locations.
- § In some places the sidewalks are narrow or non-existent.
- § There are few, if any, pedestrian scale amenities.
- § Curb cuts are poorly planned, too frequent, and compromise safety.

General Observations:

- § North Olmsted does not have a strong civic focal point or traditional city center.
- § Currently, the mall serves as the City focal point.
- § The assortment of various newspaper/advertisement boxes adds to visual clutter.
- § Some medians are in need of landscaping and others lack uniformity, consistency, and good maintenance.
- § At various sites, plantings out of scale – too big or too small
- § Highway interchanges are large in scale and appear uninviting. Most major intersections lack any community identification.
- § There is no sense of arrival to the City from any direction.

Fundamental Recommendations

A number of fundamental principles need to be taken into consideration as North Olmsted invests in its retail district. These principles represent a set of shared ideals and goals that drive the Master Plan recommendations. They result from broad consensus reached between various groups, including the public, stakeholders, and City leaders and staff. These principles are used to provide criteria for measuring the success or appropriateness of urban design and planning proposals, both within the Master Plan, and for future planning efforts in the district. In response to these fundamentals, specific action oriented goals have been formulated to help the City achieve the objectives. By accomplishing the following goals, the City seeks to improve the quality of retail district aesthetics as well as strengthen and support economic growth, development, and management of the district.

FUNDAMENTAL 1: *Different scales warrant different design solutions.*

There is a wide variety of different retail establishments within the retail district. From small converted single family residences to large regional malls, the scale of the structures varies greatly. Some areas have buildings that are close to the street while others are separated by large, expansive parking lots. It is important for the streetscape treatments to respond accordingly.

Goal #1: Organize the overall retail district into smaller sub districts to allow different approaches to the unique conditions of each area.

- § East District
- § Central District
- § West District
- § Civic District
- § Historic District

Goal #2: Adopt design guidelines to assist developers in understanding the appropriate response to development within the district at the individual property level.



Small scale retail



Large scale, big box retail



Example of street/district banners

FUNDAMENTAL 2: *Street corridors present opportunities to incorporate unifying elements and special treatments.*

There is a variety of scales, materials, uses, and setbacks along North Olmsted's retail corridors. Opportunities should be identified to utilize elements that tie together the corridor or sections of the districts, which will help unify and create a sense of order.

Goal #1: Identify and enhance significant gateways and approaches in the retail district.

- § Main gateways from I-480 interchanges
- § Secondary gateways at City borders

Goal #2: Accommodate and encourage art within the public realm.

Goal #3: Identify opportunities for implementing unifying elements and creating special treatments along the corridors.

- § Street banners City-wide or by district
- § Site furnishings
- § Edge pavers
- § Signage

Goal #4: Identify locations, such at critical intersections, where public focal points can be created.

- § Brookpark Road and Lorain Road triangle
- § Civic focal points at and/or around Dover Center, Butternut Ridge, and Lorain Roads



Example of street banner, flag, and streetlight combination



Example of public art

FUNDAMENTAL 3: *High quality streetscapes are the result of public and private cooperation.*

Many different elements contribute to a high quality streetscape environment. All landowners within a given district should be given the opportunity to work together to improve their district. These recommendations should not stop at the public right-of-way. To be successful, the configuration of buildings and parking are critical to developing higher quality streetscapes.

Goal #1: Adopt design guidelines to assist developers in understanding the appropriate response to development within the district at the individual property level.

Goal #2: Identify opportunities to acknowledge successful examples of high quality streetscape efforts.

- § Joint City/Chamber Beautification Awards
- § Adopt a Street Program

Goal #3: Investigate a self-imposed tax district to fund district-wide programs, improvement, and management.

- § Marketing
- § Site amenities
- § Beautification
- § Maintenance
- § Safety and security



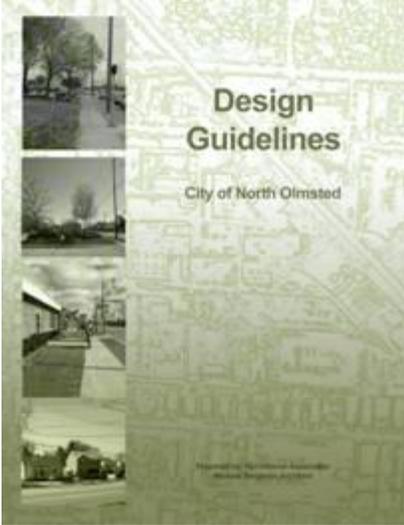
Proposed streetscape improvements



Proposed streetscape improvements



Proposed streetscape improvements



FUNDAMENTAL 4: *An improved and inviting aesthetic character along Lorain Road will lead to a positive retail experience.*

By unifying architectural forms, materials, and elements within the retail district, visual clutter can be reduced.

Goal #1: Adopt design guidelines to assist developers in understanding the appropriate response to development within the district at the individual property level.

Goal #2: Select specific materials, colors, and amenities to be used within the district(s).

Goal #3: Identify opportunities for adding new unifying elements or modifying existing elements, and creating special treatments along the corridors.

- § Street banners City-wide or by district
- § Site furnishings
- § Edge pavers
- § Signage

Goal #4: Eliminate or reduce the visual impact of elements contributing to the visual clutter.

- § Signage
- § Utility structures/poles
- § Curb cuts

Goal #5: Reduce the visual impact of parking lots.

- § Improve parking lot screening
- § Remove pavement adjacent to the street
- § Reevaluate parking lot design codes
- § Work towards sharing or combining parking lots



A possible unifying element for the Civic District



Example of a well maintained landscape

FUNDAMENTAL 5: *Appropriate long term management of the streetscape within each district is critical.*

High quality streetscapes, public improvements, and private investments not only require initial capital investment, but also a continued investment in maintenance. A coordinated effort should be made with the City and the retail establishments to understand the appropriate level of maintenance and responsibility for the corridor.

Goal #1: Create streetscapes using materials that are appropriate for specific conditions and provide long term durability.

- § Salt tolerant
- § Wind resistant
- § Water and snow resistant
- § Sun/heat resistant
- § Vandal resistant
- § Streetscape elements and/or plant material is of the appropriate scale for given location

Goal #2: Clarify the responsibility of maintenance between City and private land owners.

Goal #3: Adopt design guidelines to assist developers in understanding the appropriate response to development within the district.

Goal #4: Investigate a self-imposed tax district to fund district-wide programs, improvement, and management.

- § Marketing
- § Site amenities
- § Beautification
- § Maintenance
- § Safety and security

II. Districts

The following City districts have been delineated based upon the given architectural character, style, location, and economic use of areas with the city of North Olmsted. The creation of these districts enables the formation of smaller groups within the City. This method of organization should help local businesses to work together to better deal with issues unique to their district. The creation of the districts also allows the City to address specific issues relating to unique opportunities within the districts. Design styles utilized should create an identity for each individual district and coordinate with other districts to unify the City as a whole.

- **East Retail/Commercial District**
- **Central Retail/Commercial District**
- **West Retail/Commercial District**
- **Civic District**
- **Historic District**

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A. East Retail/Commercial District:



Small businesses exist in converted single family homes.



Fish Furniture is an example of the newer and larger business redeveloping in the East District.



Fire Station No. 1



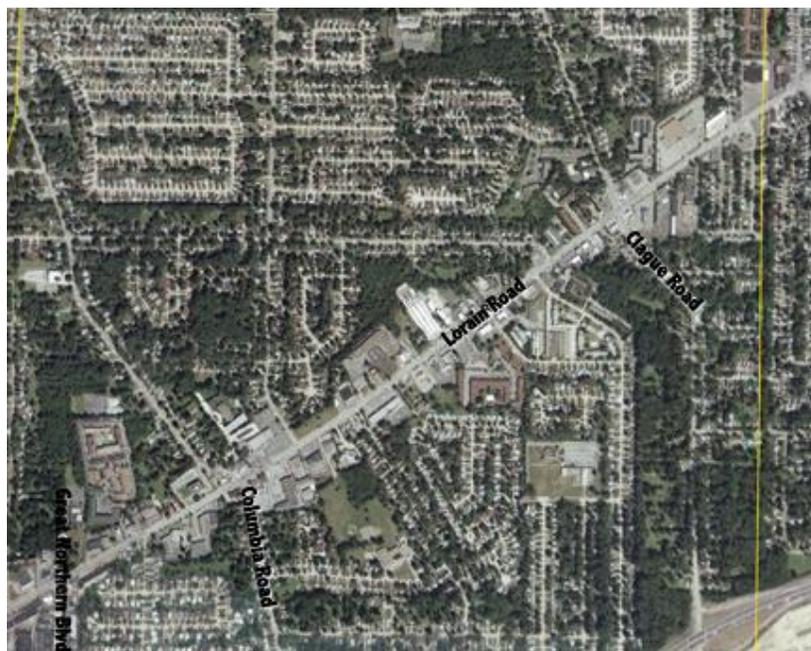
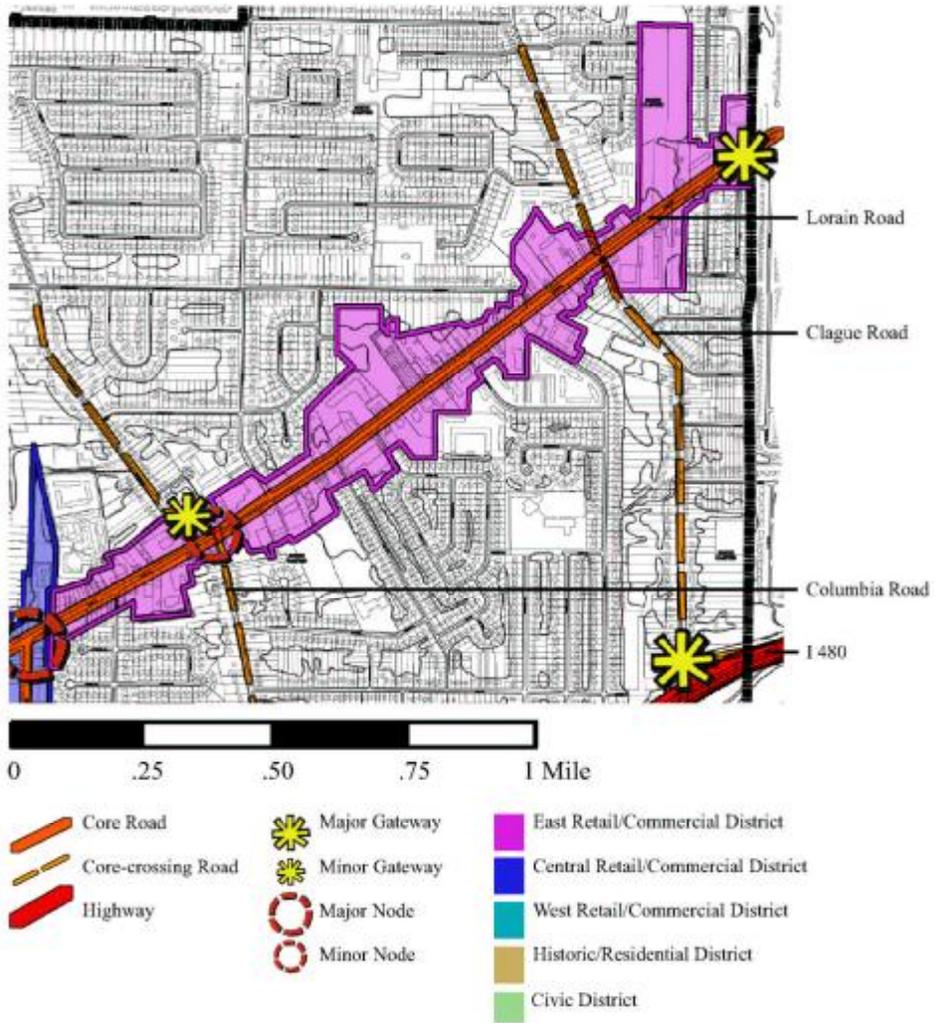
A street scene typical of the East District

The East District extends from the eastern City border along Lorain Road to the intersection with Great Northern Boulevard. This district is defined by its wide variety of retail establishments. Businesses range from older existing single family homes that have been converted into small business to large retail centers and hotels. Examples of such small businesses include beauty salons, auto body shops, and insurance agencies. The larger stores include Auto Zone, CVS, Sears, Sofa Express, and several gas stations. Also located in this district is Fire Station #1. In older developments, the lot size is generally quite small with a short setback distance from the street creating a small scale feel within the district. It would be desirable to maintain the unique look and character of this district when new development occurs.



A typical section along Lorain Road: This image shows the intersection of Lorain Road and Columbia Road. This image illustrates small setbacks and arrangements of buildings common in the East District.

East District



An overall view of the East District

B. Central Retail/Commercial District:



Looking across Great Northern Blvd, it is easy to see the excess pavement and parking found in the Central District.

Boundaries for the Central District are Brookpark Road and Columbia Road to the east, Great Northern Boulevard and I-480 to the south and Dover Center Road continuing on the north side of Lorain Road to Butternut Ridge Road. The Central District is distinct from any other district. The large number of big-box stores, strip malls, and the mall itself create a unique character. The lot sizes are much larger than those of the East District and parcel boundaries are sometimes indistinguishable from one another, giving the district a distinct large scale feel. A goal for this district would be to work together with Westfield Corporation and Developers Diversified to help create unity and promote an identity for the Central District.



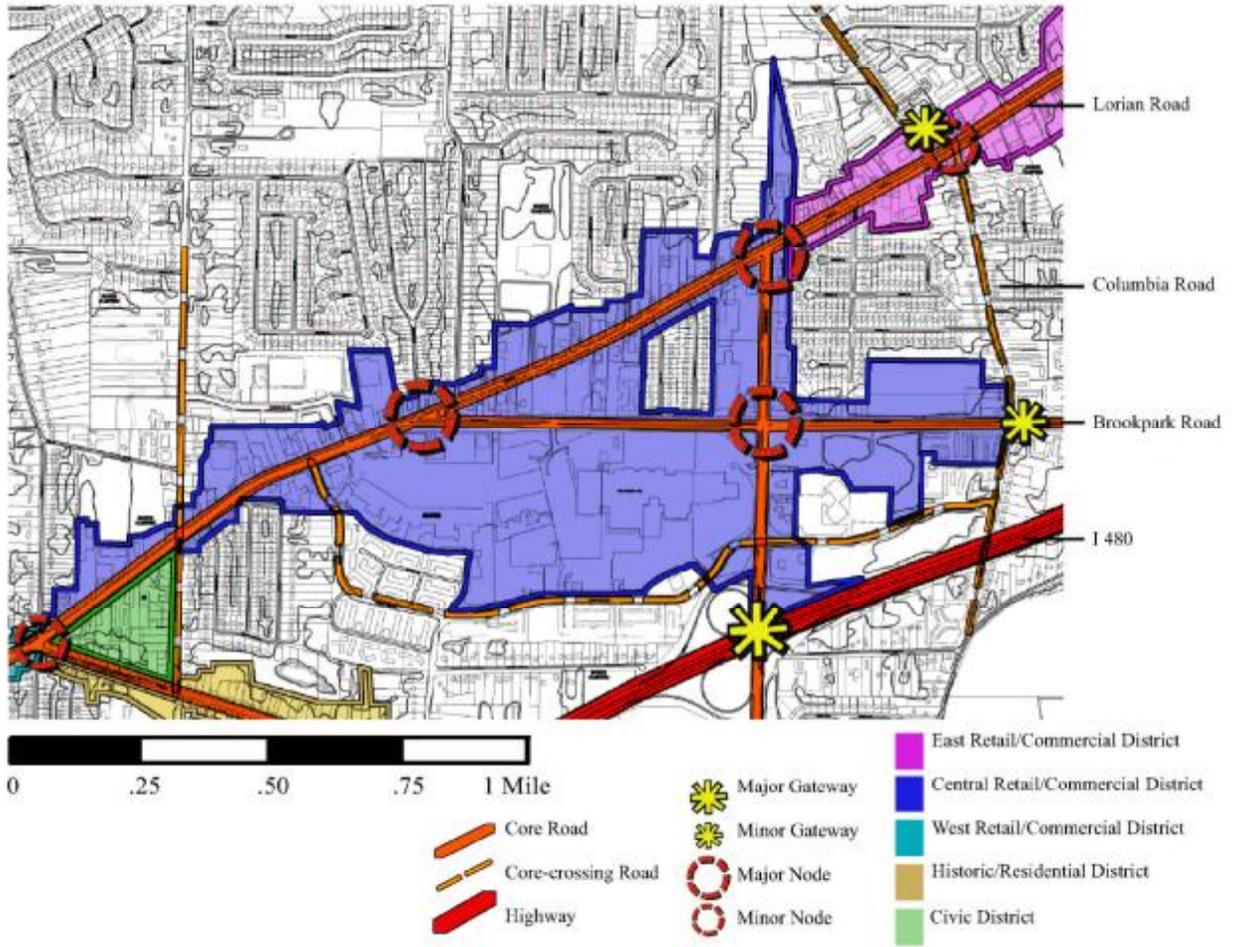
A typical scene in the Central District



Visual clutter in the Central District

A typical look in the Central District: This image shows the intersection of Lorain Road and Brookpark Road and Great Northern Plaza. Evident in this district is a lack of greenspace.

Central District



An overall view of the Central District

C. West Retail/Commercial District:

This West District is a more newly developing district. Along Lorain Road in the eastern part of this district, large residential properties exist. Farther west, there are light industrial developments and a significant amount of green space. Setbacks are generous in the West District and this is its distinguishing characteristic. Examples of businesses in this district are warehouses, industrial corporate centers, landscaping companies, nurseries, and churches. The dense commercial development of the East and Central Districts has yet to expand to the West District. This represents both an opportunity to better control development and the problem of increasing development pressures from the Crocker-Stearns extension and improved highway accessibility extending the negative aspects of sprawl.



A view of the generous setbacks characteristic of the West District

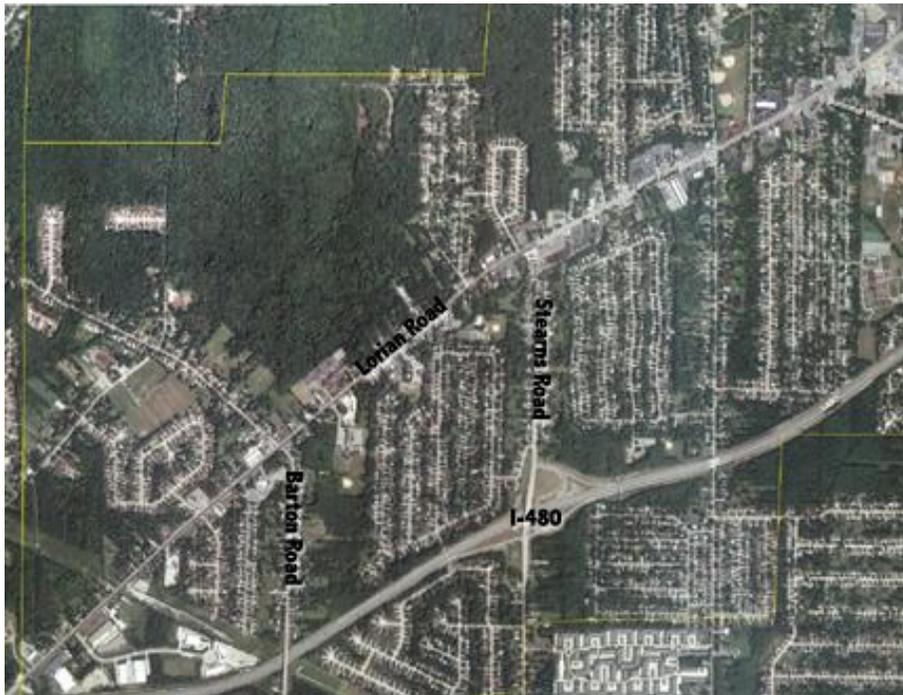
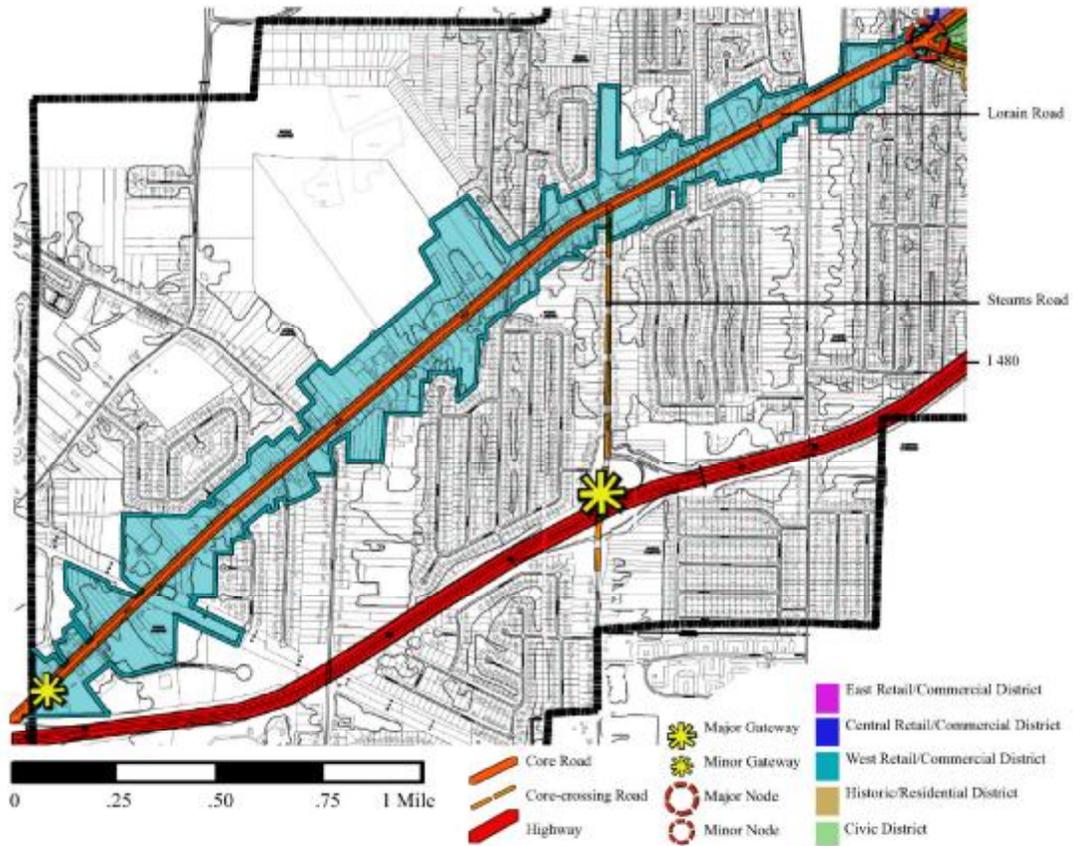


A typical scene in the West District which has a more spread out and spacious feel



A typical view of the West District: This image shows the predominance of single family residences with generous setbacks and existing green space.

West District



An overall view of the West District

D. Civic District:

The City of North Olmsted lacks a traditional downtown, such as the type of civic square in the cities of Medina or Hudson. The existing municipal buildings in North Olmsted do not have a presence along the major roads. The addition of the new library begins to create a civic presence along Lorain Road. By establishing a civic district, the City will have an opportunity to consider redeveloping their municipal facilities in order to create an improved civic presence within the community.

The Civic District is a triangle of land bounded by Lorain Road, Butternut Ridge Road, and Dover Center Road. City Hall, the new library, police station, and Old Town Hall are all located within the Civic District. The City should work to consolidate civic uses in the district, improve visibility of the district, especially along Lorain Road, and enhance the overall visual appeal of the district. A goal for this district could be that, over time, the Civic District becomes a unified public campus.



North Olmsted library viewed from Lorain Road.

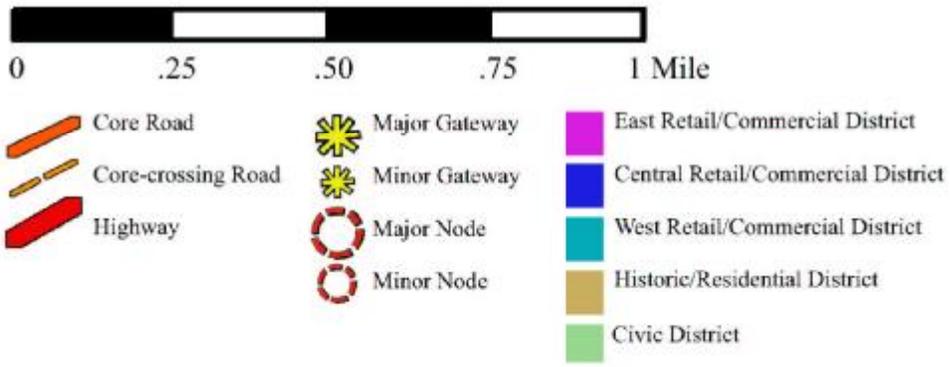


North Olmsted library viewed from the parking lot facing Butternut Ridge Road.



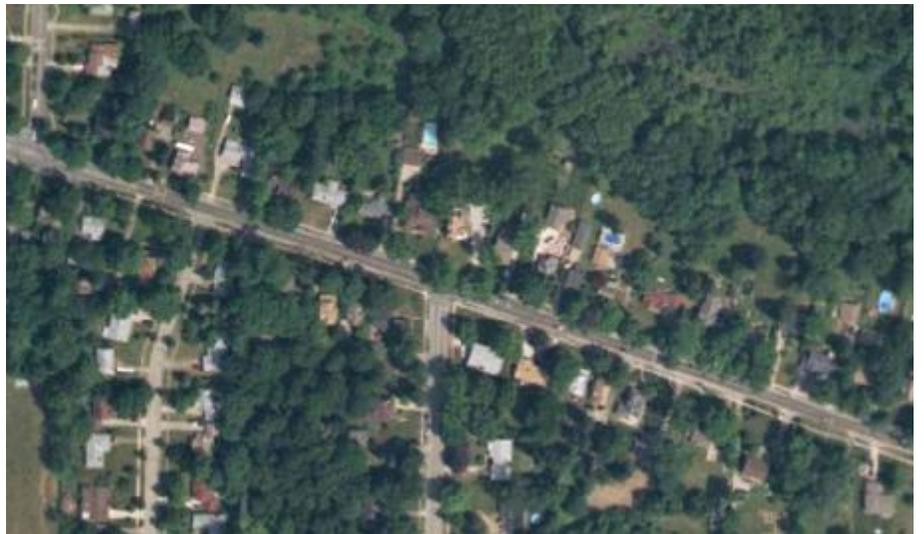
This shot of the Civic District was taken during construction for the new library.

Civic District



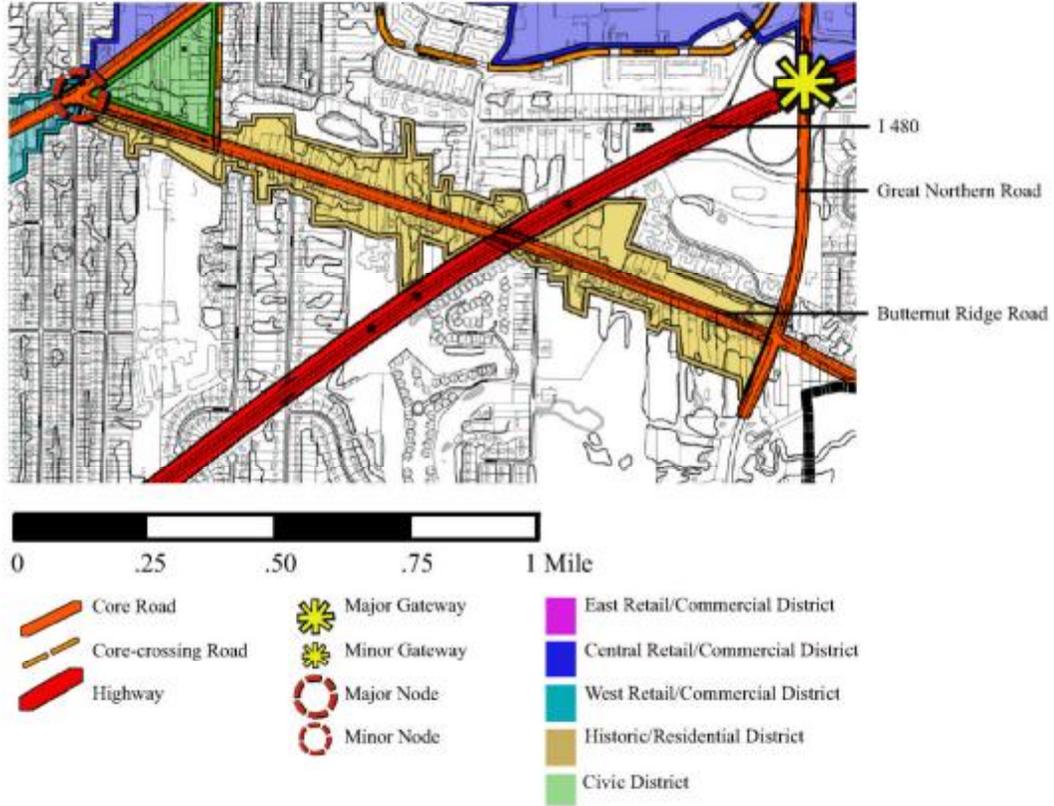
E. Butternut Ridge Historic District:

The Historic District is comprised almost exclusively of residential homes, many of which are of a historic nature. This district extends along Butternut Ridge Road from Great Northern Boulevard on the east to Porter Road on the west. The City should maintain the character and scale of this district. Additions and renovations to existing structures should follow historic guidelines to maintain and enhance the quaint, small town quality it has now.



The historic/residential district along Butternut Ridge Road is almost exclusively residential with average setbacks.

Butternut Ridge District



An overall view of the proposed Historic District

III. Gateways/Gateway Features

A. Major and Minor Gateways

The main entrances to the City are perfect locations to enhance the identity of the City. Such elements foster a stronger sense of community, as well as helping with wayfinding. Since the majority of traffic in the City is vehicular, in order to be effective, any feature installed should take vehicular speed into consideration and be designed to the appropriate scale. The following are locations where opportunities for gateway treatments and/or other entry features into the City exist:

1. Major Gateways:

- a. I-480 and Great Northern Boulevard
- b. I-480 and Clague Road
- c. I-480 and Stearns Road

2. Minor Gateways:

- a. Columbia Road at Lorain Road
- b. Brookpark Road at Columbia Road
- c. Fairview Park border
- d. North Ridgeville border



There is no clear demarcation of where Fairview Park ends and North Olmsted begins.



Entering the City from the east along Brookpark Road



Sketch showing entrance landscaping at the off-ramp of I-480 onto Great Northern Blvd.



I-480 and Great Northern Blvd. interchange – one of the biggest and most used gateways into the City.

B. Gateway Features



Proposed median and entry plantings

The following are examples of what could be done at the various gateways throughout the City.

1. Planting

- a. Eye-catching, colorful plantings are a simple way to accentuate a City gateway.
- b. Good maintenance of these plantings is critical.

2. Signage

- a. City entrance signage is perhaps the most common way to mark an entrance to the City.
- b. Entry signage should coordinate with civic signage.
- c. Any entry signage should be distinct from other entry signage of neighboring cities.



Example of street signage and secondary street signage

3. Columns

- a. Columns can be used in conjunction with any of the other gateway features.
- b. Columns sited at the gateways can create a stronger sense of entry into the City.
- c. Columns could contain signage as well.

4. Structures

- a. Clock tower
- b. Informational sign

5. Lighting

- a. Appropriate lighting of any gateway feature helps improve its desired effect.
- b. Lighting is especially necessary if any signage is used.

6. Alternate Options

- a. Any other option properly submitted and deemed appropriate by the City.



The Rocky River clock tower

IV. Public Spaces/Nodes

A. City Parks

1. City parks should have a unified signage system easily understandable by its users. Sign should display similar information: usage areas, rules and regulations, hours of use, etc.
2. Any possible connection between parks or other public open spaces is highly encouraged.

B. Nodes

Utilize nodes to create a sense of place for major intersections throughout the City. These locations are high volume, high traffic areas that get a lot of use. Elements placed at these locations would add a highly visible identifying and unifying element to the location. Additionally, elements placed at these nodes will help with wayfinding throughout the City.

- § Brookpark Road and Great Northern Road
- § Great Northern Boulevard and Lorain Road
- § Brookpark Road and Lorain Road (triangle)
- § Butternut Ridge Road and Lorain Road

1. Ways to enhance and accentuate nodes:

- § Street/district banners
- § Landscaping
- § Signage
- § Structures (i.e., columns, walls, clock tower)
- § Site amenities

2. Any element placed at a node should not hinder safety or maintenance of the area.



A view of North Olmsted Park



A view of the existing triangular island at the intersection of Brookpark Road and Lorain Road – one of busiest areas in the City



Potential civic site at Brookpark Road and Lorain Road triangle

V. Streetscape Elements

Correctly utilizing streetscape elements can be one of the most effective ways to help create unity and spatial definition within a city. Elements should be easy to maintain and not obstruct pedestrian or vehicular traffic flow. Elements should be to scale with their surroundings. For example, any element should be designed with either vehicular scale or pedestrian scale in mind, depending on location. Streetscape elements could be the same throughout North Olmsted or varied slightly between districts to better define each district.

For other information regarding site design please see the North Olmsted Design Guidelines.

A. Elements

1. Public Art

- a. Art is highly encouraged as part of the overall site concept.
- b. Any art placed in the City should be of an appropriate nature and fit within its given district's character.
- c. Permanent art should be constructed of long lasting durable materials to withstand being exposed to the elements year-round.
- d. Art should be such that opportunities for vandalism are kept to a minimum.



2. Planters

- a. Planters can very effectively used as a streetscape element. If properly sited and maintained they create year-round interest. Planter style and color should be appropriate given the specific site as well as the district. Planters can be used as a means of controlling traffic, defining spaces and uses, or enhancing aesthetic value among other uses. Planters placed on walkways should not create congestion or block pedestrian traffic.
- b. Ground plantings can also be effectively used.
- c. Plantings are covered in the North Olmsted Design Guidelines manual.
- d. See planter specifications on page 31



3. Seating

- a. Public seating is another effective means of creating unity throughout the City.
- b. A consistent seating style should be used and located in similar ways throughout the City.
- c. Seating should be located where it will actually be used instead of just seen from the roadway. Civic areas and parks are excellent locations for seating opportunities.
- d. See seating specifications on page 32



4. Trash Receptacles

- a. Trash receptacles can be the most used streetscape element.
- b. Trash receptacles should be of a consistent design throughout the City for ease in locating, maintenance, and/or replacement. They should be conveniently located for pedestrian traffic and be easy to maintain.
- c. See receptacle specifications on page 33





5. **Newspaper Boxes**

- a. Newspaper and advertisement boxes serve an important public function. However, if not located properly, they can become visually unappealing as well as create a public safety hazard.
- b. All boxes must conform to the City specifications.
- c. See newspaper specifications on page 34



6. **Bike Racks**

- a. Bike racks can facilitate a lesser dependence on automotive travel within the City.
- b. A consistent style of bike rack should be used and located in similar ways throughout the City.
- c. Bike racks should be located where they will actually be used instead of just seen from the roadway. Civic areas and parks are excellent locations for bike racks.
- d. See bike rack specifications on page 35

7. **Utilities**

- a. Utilities should be located underground.
- b. The number of utility poles should be minimized and/or combined whenever possible.

B. Color Discussion

1. **White:** White creates a simple look. It is an easy color to match either over time or between different manufacturers. However, white shows dirt very quickly. Also, white will show damage and rust.
2. **Black:** Black blends into the environment more than white. It matches other site furniture well. It does not show dirt but could show salt spray. Black tends to fade over time. Also, black creates an elegant appearance.
3. **Bronze/Brown:** Bronze/brown blends into the environment best. It matches other site furniture well. This color hides dirt well and will show less fading over time. Bronze brown will show salt spray as well.
4. **Green/Navy/Other:** Less common colors may require “custom colors” which could require premium prices. These colors are more difficult to match and could also be subject to fading over time. These colors may not match other site furnishings.

C. Materials Discussion

1. **Durability:** It is important that streetscape elements be long lasting and strong. Durable elements will keep a more appealing aesthetic for a longer period of time adding to an improved streetscape. Stainless steel elements will be more durable than wood, concrete or plastic elements.
2. **Clean, classic look:** Streetscape elements selected should not contribute to the visual clutter that currently appears along the retail corridor. By selecting elements with simple, classic designs it is possible to create a more unified streetscape. Also, it is easier to replace, match and coordinate elements of a similar style.
3. **Maintenance free:** Stainless steel streetscape elements are the best choice. They do not require staining, do not warp, are resistant to rust and fading, and will need to be replaced much less than wood, concrete, or plastic elements.

Planter Specifications

Specs: All planters for North Olmsted should meet the following qualifications:

Material: Steel

Design: Coordinate with benches and trash receptacles

Acceptable:



Unacceptable:

Any wood, concrete, or plastic planters will be considered unacceptable. Wood requires a great deal of long term maintenance.



Suggested manufacturers include:

Keystone Ridge: www.keystoneridgedesigns.com

Wabash Valley: www.wabashvalley.com

Dumor Site Furnishings: www.dumor.com

or approved equal

Colors: Black

Seating Specifications

Specs: All seating for North Olmsted should meet the following qualifications:

Material: Steel

Design: coordinate with benches and trash receptacles

Acceptable:



Unacceptable:

Any wood, concrete, or plastic bench will be considered unacceptable to the City.



Suggested manufacturers include:

Keystone Ridge: www.keystoneridgedesigns.com

Wabash Valley: www.wabashvalley.com

Dumor Site Furnishings: www.dumor.com

or approved equal

Colors: Black

Trash Receptacle Specifications

Specs: All trash receptacles for North Olmsted should meet the following qualifications:

Materials: Galvanized material, stainless steel hardware, and polyester powder covering.

Design: Heights should be uniform height of ____ with a consistent, uniform shape and a ____ gallon capacity.

Acceptable:



Unacceptable:

Any wood, concrete, or plastic trash receptacle will be considered unacceptable to the City.

No trash receptacles with designed flat tray return tops will be acceptable. Receptacle design should not be fancy or intricate as this may be more difficult to match, repair, or replace.



Suggested manufacturers include:

Keystone Ridge: www.keystoneridgedesigns.com

Wabash Valley: www.wabashvalley.com

Dumor Site Furnishings: www.dumor.com

or approved equal

Colors: Black or bronze/brown

Advertisement/Newspaper Box Specifications

Specs: All newspaper boxes for North Olmsted should meet the following qualifications:

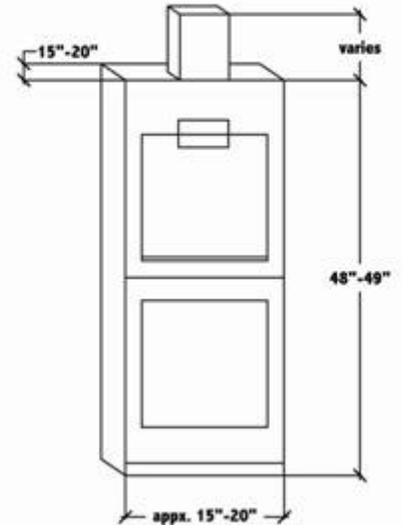
Heights should be uniform 48"-49" with a consistent, uniform shape.

The width of the boxes may vary slightly between units to accommodate different sized material. Materials: Hinged 12 Gauge Armored Housing,

Acceptable:



Proposed dimensions:



Unacceptable:



Suggested manufacturers include:

Sho-Rack - 800-527-1134

www.shorack.com

or approved equal

Colors: Bronze/brown

Bike Rack Specifications

Specs: All bike racks for North Olmsted should meet the following qualifications:

Materials: galvanized material, stainless steel hardware, and polyester powder covering.

Acceptable:



Unacceptable:

Any wood, concrete, or plastic bike racks will be considered unacceptable to the City.



Suggested manufacturers include:

Columbia Cascade: www.timberform.com

Dumor Site Furnishings: www.dumor.com

or approved equal

Colors: Black or any other color that coordinates with the architecture of the main building.

VI. Wayfinding

A good wayfinding system is important to a successful city. Wayfinding helps people navigate through a community or to specific locations. Wayfinding can be found in many different forms and can be a very useful tool in unifying the City.

A. Types of Wayfinding

1. Street Signs

- a. Above all, street signs should be highly visible to both vehicular and pedestrian traffic.
- b. Street signs should clearly and concisely communicate information.
- c. Street signs should be uniform throughout a single district and, in an obvious way, coordinated throughout the entire City.



2. District Signage

- a. The purpose of district signage is help with wayfinding in the City.
- b. District signage could be a distinctly unique signage system found solely in one district or a consistent signage system used in all districts that identifies the given district.
- c. District signage can also serve to unify the City by carrying a theme through all districts.



3. Street Banners

- a. Street banners are a very visible way to infuse the streetscape with color year-round.
- b. Banners can be decorative or relay information. They can also serve to delineate different districts.
- c. Banners can detract from utility poles and roadside clutter.



- 4. **Civic Signage**
 - a. Standardized civic signage
 - b. New graphics
 - c. New/revised signage standards
 - d. Civic building signage



- 5. **Other**
 - a. Monuments
 - b. Park and recreation signage
 - c. Directional signage
 - d. Safety signs
 - e. Informational signs

